

Bloomdale Employment Precinct Urban Design Framework

Bloomdale Employment
Precinct UDF
22.05.2020



CITY OF
MELTON
MELTON PLANNING SCHEME

Urban Design Framework approval in accordance with
Clause 37.07 - Schedule 5 - Diggers Rest Precinct Structure
Plan of the Melton Planning Scheme.
Page 1 of 29

Roger Susic
Signature of Responsible Authority



Document Control

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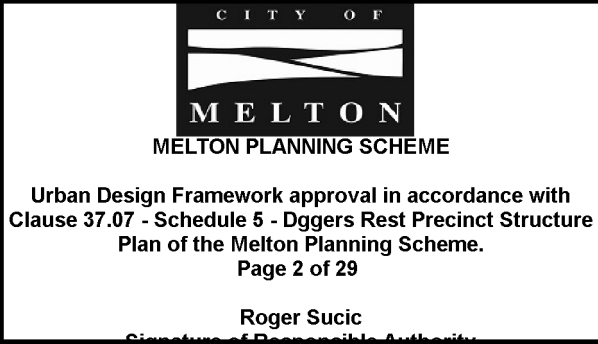
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Image representing good architectural outcome.

Introduction

Bloomdale is approximately 35km north-west of Melbourne's Central Business District and can be accessed via the Calder and Tullamarine Freeways from Melbourne. The Urban Design Framework (UDF) has been prepared for the proposed employment precinct, Lot J and Lot I, License Road within Bloomdale Estate, Diggers Rest.

01 Purpose and Strategic Context

This UDF has been prepared to ensure that the design of buildings and spaces within the Employment Precinct will support the realisation of the vision, strategic directions and desired outcomes of Diggers Rest PSP.

The UDF will guide the future industrial / employment subdivision such that it integrates with the residential neighbourhood through appropriate urban design treatments. The UDF will inform the use, built form and public realm within the employment precinct by providing clear objectives and standards for developers.

Melton Planning Scheme gives effect to Diggers Rest PSP through the application of zones, overlays and policy requirements. The Diggers Rest PSP is included in the Urban Growth Zone (Schedule 5), and the employment precinct falls under the applied Commercial 2 Zone (C2Z). Land use and development within the precinct must include the provision of Melton Planning Scheme:

- To encourage commercial areas for offices, appropriate manufacturing and industries, bulky goods retailing, other retail uses, and associated business and commercial services.

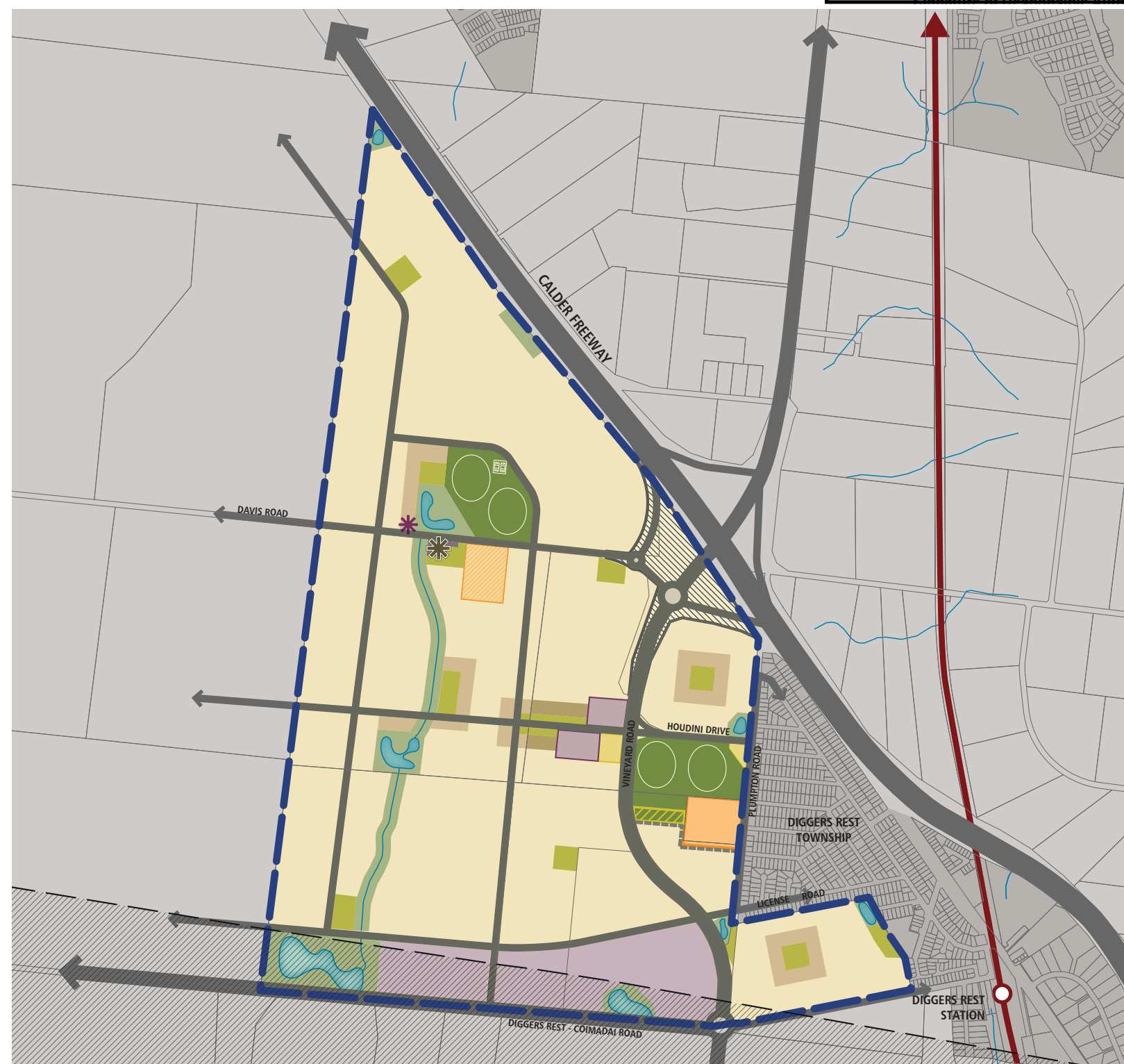


Figure 1 | Diggers Rest Precinct Structure Plan gazetted March 2012

02 Regional context

The Bloomdale Employment Precinct covers 11.50ha and is located within City of Melton, approximately 35km north west of the Melbourne CBD, in the suburb of Diggers Rest.

Public transport access

The nearest train station is Diggers Rest Train station approximately 2.0km from the site

Community and retail uses

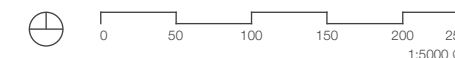
The closest commercial town centre is currently located in and around Sunbury approximately 6.0km from the site. The Bloomdale Town Centre is expected to be delivered by 2024. There is a primary school approximately 750m from the site.

Sports, recreation and leisure amenities

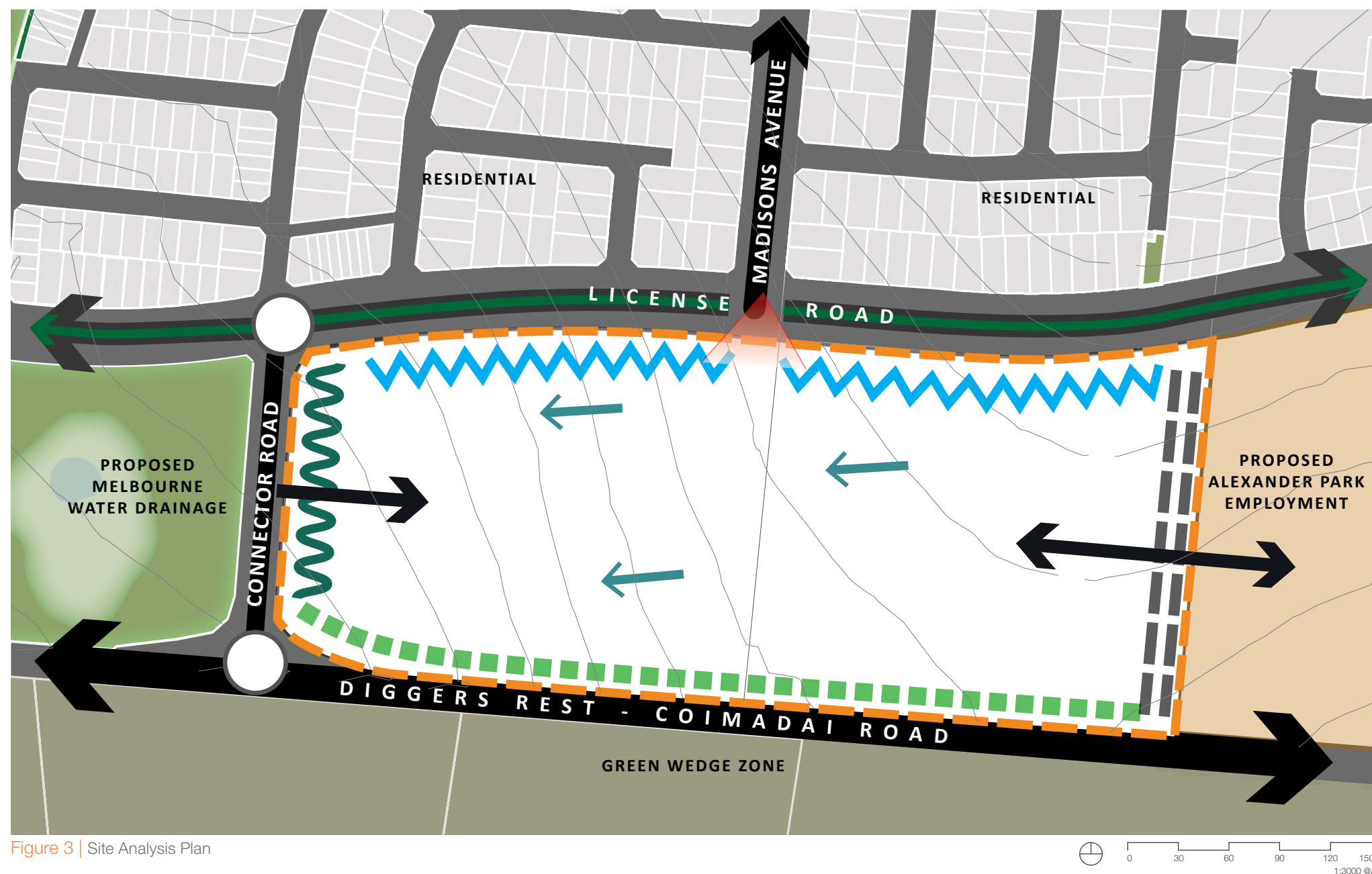
The closest recreation facilities are currently located in Bloomdale to the north of the site.



Figure 2 | Regional Context Plan (image source Nearmap aerial photo 08/10/2019)



03 Site analysis



The employment precinct to which this UDF applies is within the Diggers Rest Precinct Structure Plan as shown in Figure 1. The site is bound by License Road and a residential subdivision is currently being developed to the north with a future employment subdivision proposed to the east. To the south is Diggers Rest – Coimadai Road, beyond which is land zoned Green Wedge. To the west of the site is future drainage and wetlands reserve separated by a north south connector road.

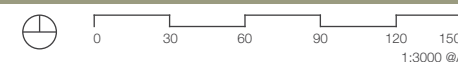
The employment precinct has an area of approximately 11.5ha. The land is generally flat and drains naturally to the west except for the south-east corner near the future retarding basin where 3-4m of fall across part of the site exists.

This UDF has been prepared in line with the requirements set out in the Diggers Rest PSP.

Legend

- Framework Plan boundary
- ~ Residential Interface
- ~ Melbourne Water Drainage Interface
- Green Wedge Interface
- Proposed Alexander Park Employment Interface
- < Madisons Avenue Visual Connection
- Potential Connection
- Slope
- | Contours (1m interval)

Figure 3 | Site Analysis Plan



04 Urban Design Framework Plan

Vision

“To create an Employment Precinct with articulated built form, attractive and engaging streetscapes that integrate with the existing Diggers Rest Township based on best practice sustainability and design principles.”



Project vision image



Project vision image



Project vision image



Project vision image

04 Urban Design Framework Plan

Legend

- Framework Plan Boundary
- Surrounding road network
- Roundabout
- Connection to Alexander Park Employment
- Shared path network
- Vehicle Access C
- Residential Interface
- Melbourne Water Drainage Interface
- Green Wedge Interface
- Alexander Park Employment Interface
- Setback Transition
- Madisons Avenue Visual Connection
- Landscaped Reserve
- Key Gateway
- Primary Entry
- Secondary Entry
- Primary Cyclist and Pedestrian Entry
- Urban Grain

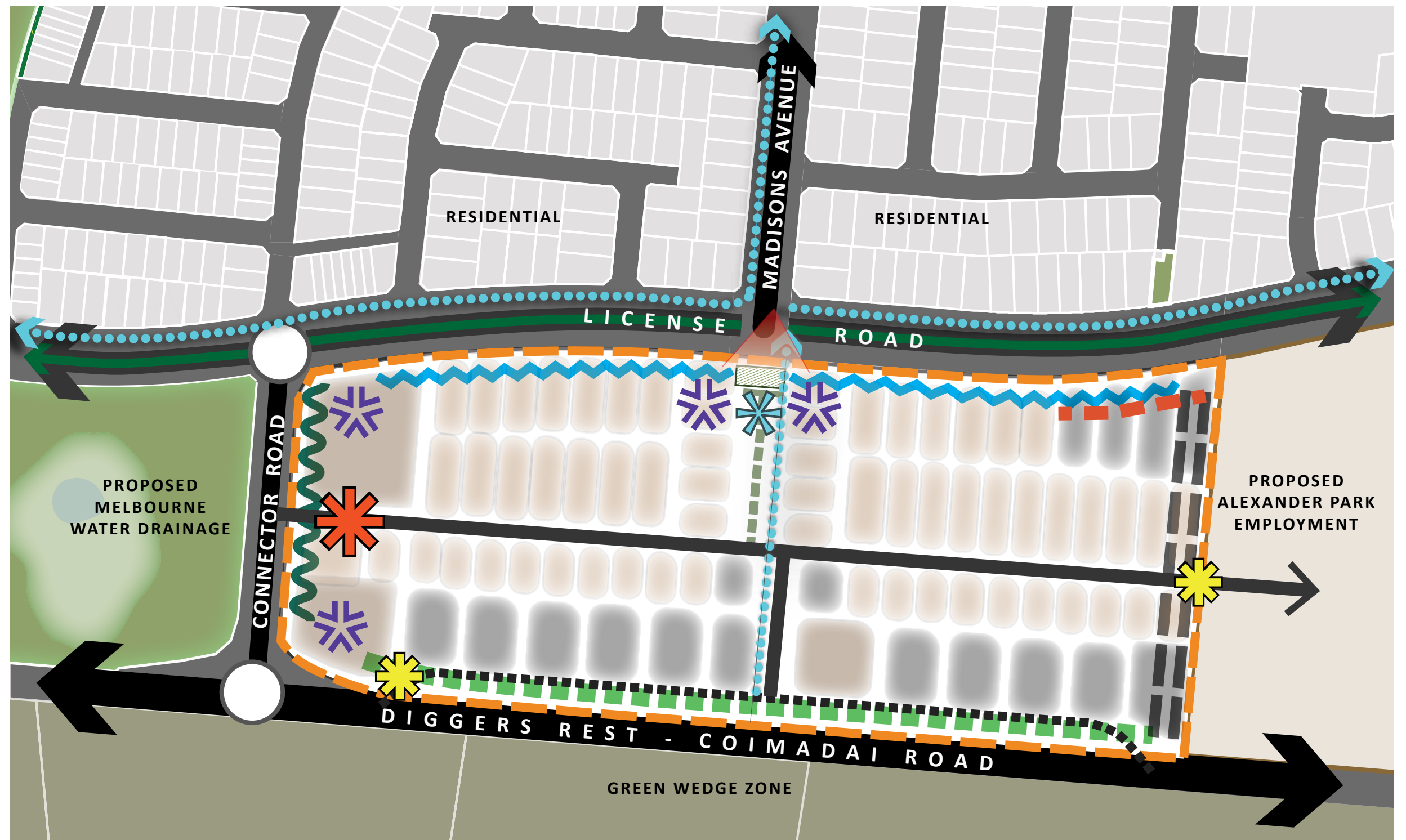
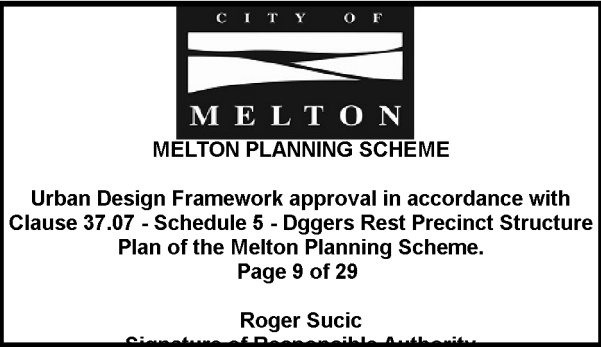


Figure 4 | Urban Design Framework Plan



04 Urban Design Framework Plan

Comments



Comments



RESIDENTIAL INTERFACE

Ensure the built form and urban grain transitions from residential to employment uses at this interface. Ensure active frontages at the interface with articulated façades, definitive rhythm of the horizontal and vertical elements and frequent doors and windows to create a distinct edge which is congruent to the residential interface.



MELBOURNE WATER DRAINAGE INTERFACE

Ensure the built form is responsive to the frontage along the reserve with activated edges providing passive surveillance.



GREEN WEDGE INTERFACE

Locate larger lots at this interface accessed by service road. Diggers Rest-Coimadai Road will potentially direct traffic to Bloomdale Estate from Diggers Rest Train Station, there is an opportunity to create a distinct identity for the precinct and frame the arrival.



PROPOSED ALEXANDER PARK EMPLOYMENT INTERFACE

Ensure the urban structure is responsive to the future Alexander Park Development, with road connection continuity.



SETBACK TRANSITION

Ensure the front building and landscape setback requirements transition across first four lots, from those in Alexander Park Development to Bloomdale Employment Precinct.



MADISONS AVENUE VISUAL CONNECTION

Ensure that the vista from Madisons Avenue does not terminate into industrial buildings at License Road interface. A well landscaped buffer is to be delivered, marking the centre of the Employment Precinct and to act as a physical connection. Gateway buildings are to frame the arrival to the Employment Precinct at this node.



LANDSCAPED RESERVE

Ensure delivery of a landscaped reserve (31m x 6m) at the intersection of Madisons Avenue and License Road to provide visual and pedestrian connection and to prevent vehicle access to the Employment Precinct from License Road.



KEY GATEWAY

Ensure architecturally well-presented built form outcomes in key locations to act as landmarks and visual markers. Ensure that the built form of the gateway sites punctuate the rhythm of the urban structure to create focal points.



PRIMARY ENTRY

Primary entry is from the north-south connector at the Melbourne Water Drainage Interface. Gateway buildings to frame and direct the incoming traffic.



SECONDARY ENTRY

Secondary entries are from the service road off Diggers Rest - Coimadai Road and from Alexander Park Development to the east.



PRIMARY CYCLIST AND PEDESTRIAN ENTRY

An entry at the Madisons Avenue termination will allow cyclists and pedestrians to traverse across the site in north-south direction and activate this connection.



URBAN GRAIN

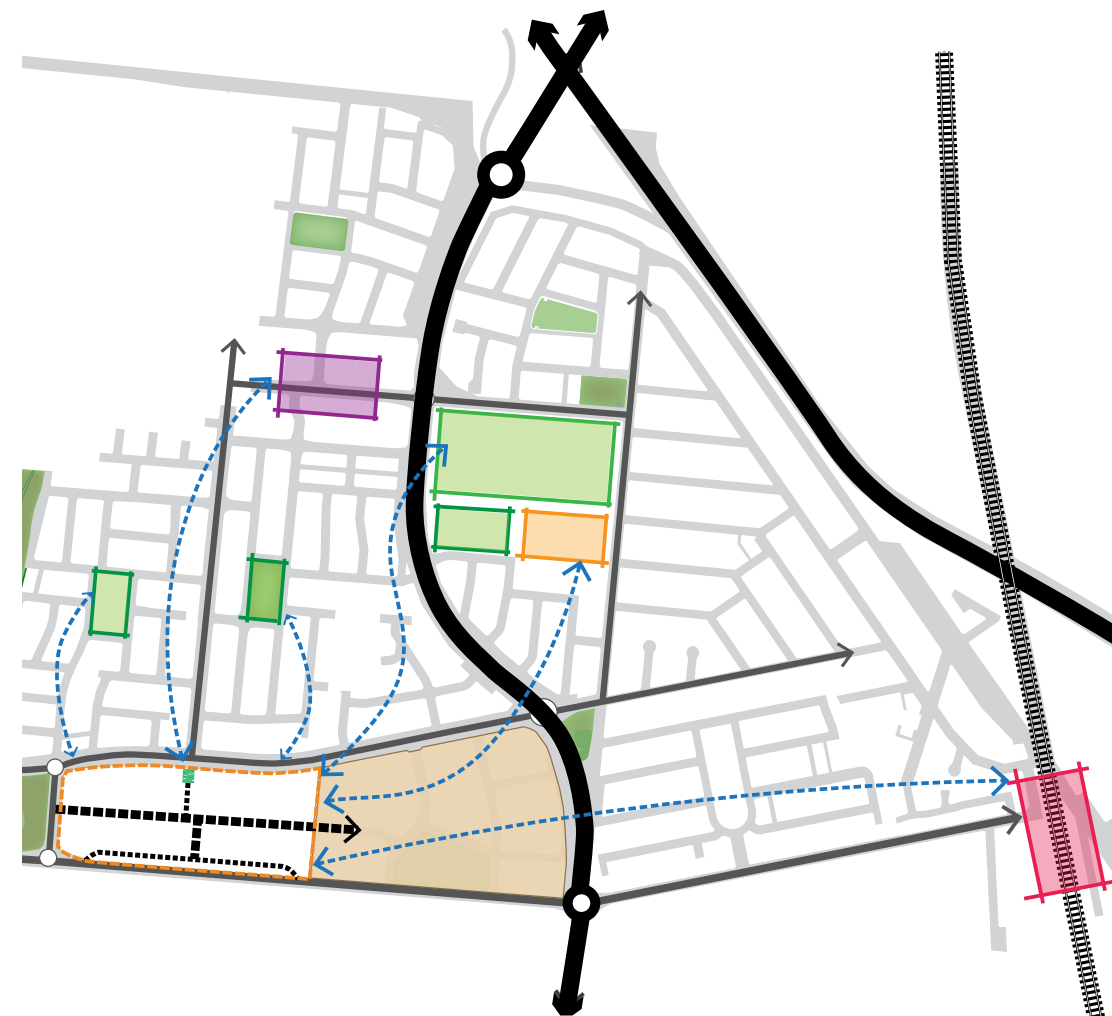
Cater for a mix of lot types to support varied business activities.

SHARED PATH NETWORK



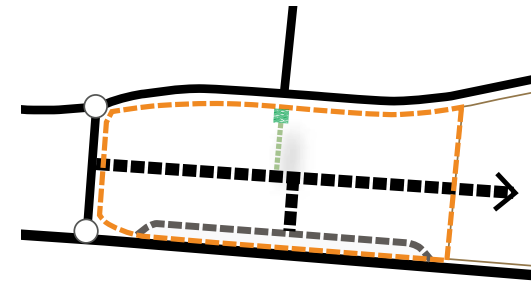
Ensure active transport link connection to the shared path network.

05 Framework Principles



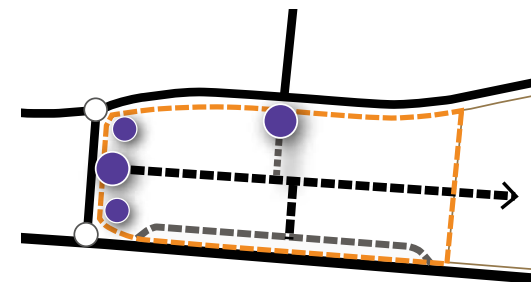
Integration

To ensure that the urban structure of the Employment Precinct reinforces the identity of Bloomdale and it integrates with the wider Diggers Rest Precinct and immediate surrounds.



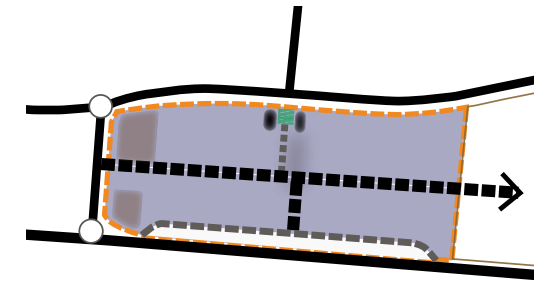
Access and Movement

To provide a permeable and safe environment for diverse modes of transport, ensuring that the footpaths and public spaces of the Employment Precinct are optimised as vibrant, safe and attractive places for pedestrian activity.



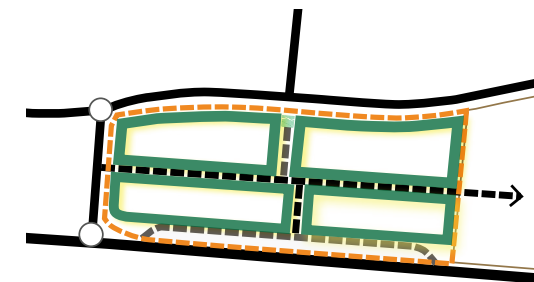
Built form

To ensure that the development within the Employment Precinct is contextually responsive in terms of its form, scale, massing, articulation, roof profile and materials, and that these design elements collectively forge a sense of place and identity for the Employment Precinct.



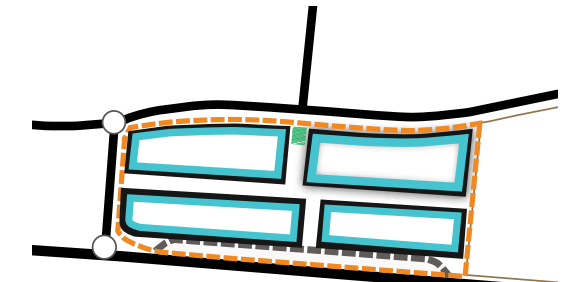
Urban Grain and Employment

To ensure proposed land use mix fosters employment opportunities and protects the amenity of surrounding residential areas, especially at the License Road interface.



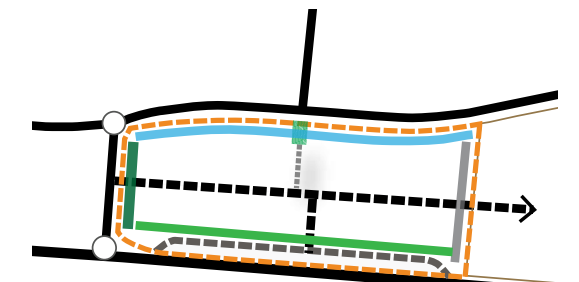
Public Realm

To create a sense of place and identity by appropriate landscaping and streetscapes.



Development Siting and Setbacks

To ensure the siting of the built form contributes to creating consistent streetscape that is visually attractive and responds to surrounding context appropriately and positively contributes to the overall character of the Diggers Rest Township.



Interface Treatment

To provide active and articulated frontages to appropriate interfaces to create a distinct identity to the Employment Precinct without compromising the overall identity of Diggers Rest Township.

06 Integration

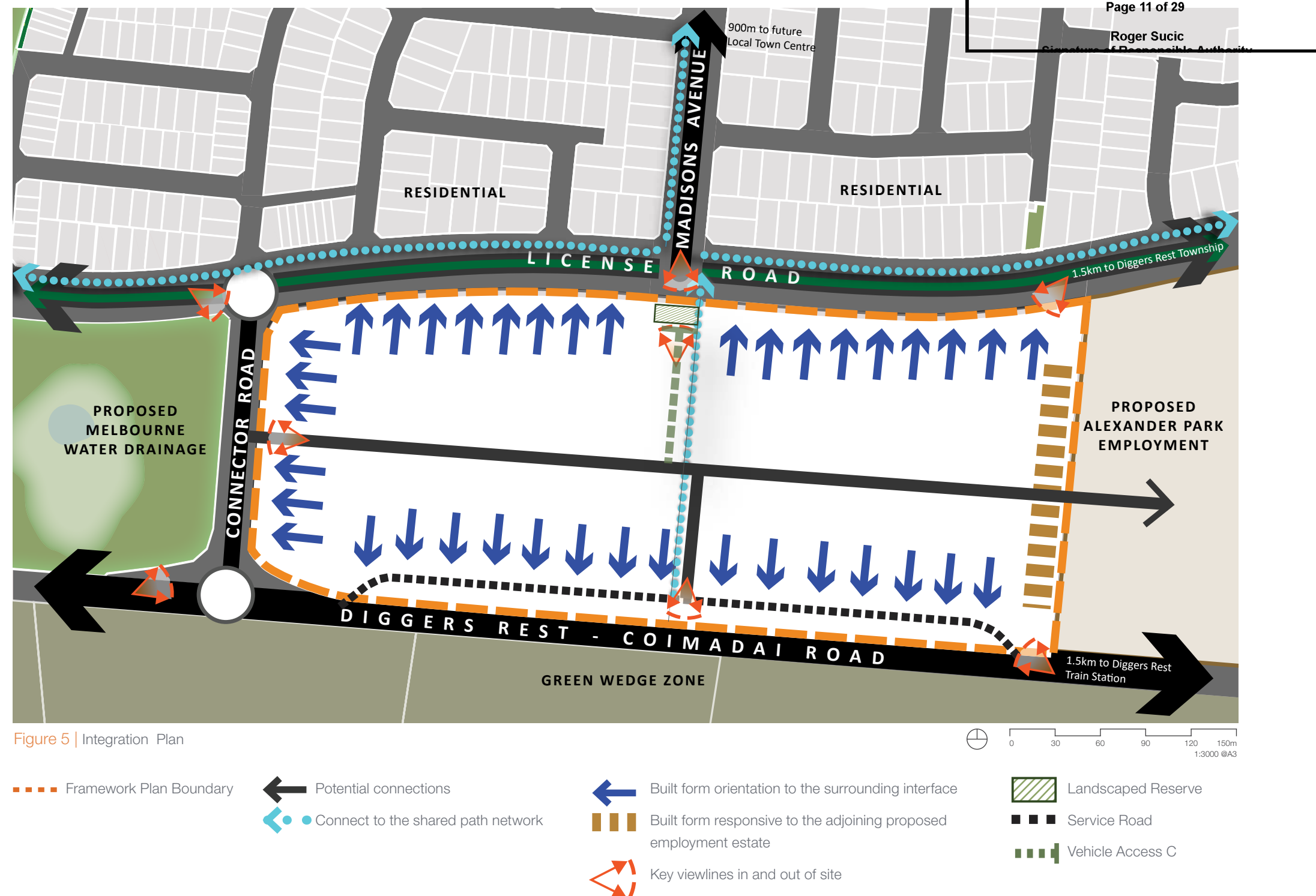
The employment precinct is bound by License Road to the north and future employment subdivision to the east. Madisons Avenue connects the precinct at the center of its northern interface with the future Town Centre. Diggers Rest – Coimadai Road forms the southern interface and connects the precinct to Diggers Rest train station.

Principle

To ensure that urban structure of the Employment Precinct reinforces the identity of Bloomdale and it integrates with the wider Diggers Rest Precinct and immediate surrounds.

Guidelines

- 6.1 Facilitate a high quality and efficient road network to ensure that the employment precinct is well connected to Bloomdale to the north and west and future employment to the east.
- 6.2 Facilitate visual connection at Madison and License Road intersection.
- 6.3 Facilitate pedestrian connection to the site at License Road and Madisons Avenue intersection.
- 6.4 Orientate built form to the northern, western and southern interfaces.
- 6.5 Ensure the built form is responsive at the interface to the residential development to the north, adjoining proposed Alexander Park Employment Estate to the east, to the Green Wedge Zone to the south and to the drainage reserve to the west.
- 6.6 Maximise cyclist and pedestrian access and connections onto existing path network at License Road.
- 6.7 Ensure development is compatible with and does not compete with existing or future retail areas proposed in the Local Town Centre to the north.



07 Access and Movement

A public transport route is envisaged along License Road with a bus stop proposed at the north west interface. A key shared path route is located along License Road (northern edge) and connects Bloomdale with existing Diggers Rest Township.

Principle

To provide a permeable and safe environment for diverse modes of transport, ensuring that the footpaths and public spaces of the Employment Precinct are optimised as vibrant, safe and attractive places for pedestrian activity.

Guidelines

7.1 Provide a North – South connection (Vehicle Access C) aligned with Madisons Avenue, that visually connects Road A and License Road. Vehicle Access C must achieve the following outcomes:

- No vehicular connection to License Road is permitted.
- There must be a minimum distance of 31 metres between any built form on Vehicle Access C.
- An appropriately dimensioned central landscaped verge/ reserve must be delivered, with breaks for vehicular circulation and access as required.

7.2 Provide a road connection to Alexander Park Development at the eastern boundary of the site as shown in Figure 6.

7.3 Provide a service road for access to lots fronting on to Diggers Rest – Coimadai Road.

7.4 Provide a North-South road connection (Road B) between Road A and Service Road for permeability.

7.5 Provide definitive road crossing points to ensure safe pedestrian and cyclist access.

7.6 Ensure road carriageway, verge and pathway widths generally correspond with the street cross sections in this UDF.

7.7 A Traffic Impact Assessment will be required as part of the future subdivision application to determine Traffic Management and Traffic Calming devices required to ensure that traffic is conveyed through the internal and external road network safely and efficiently.

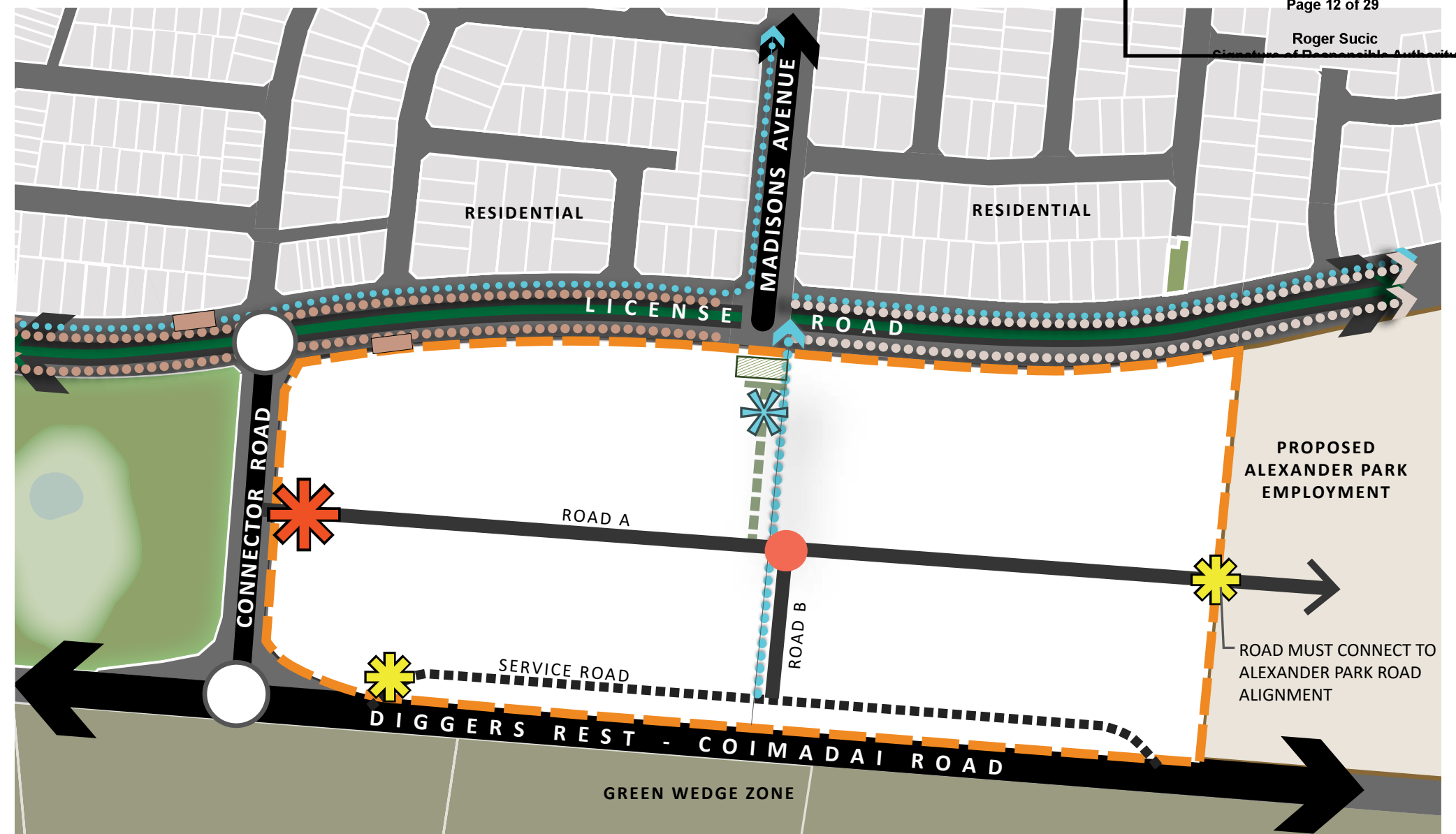
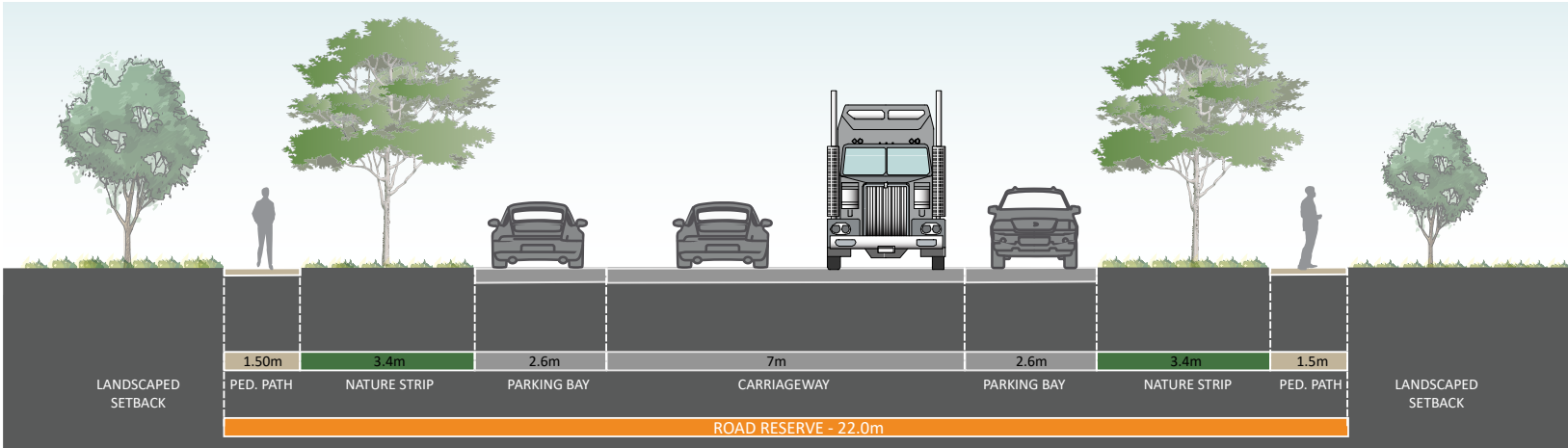


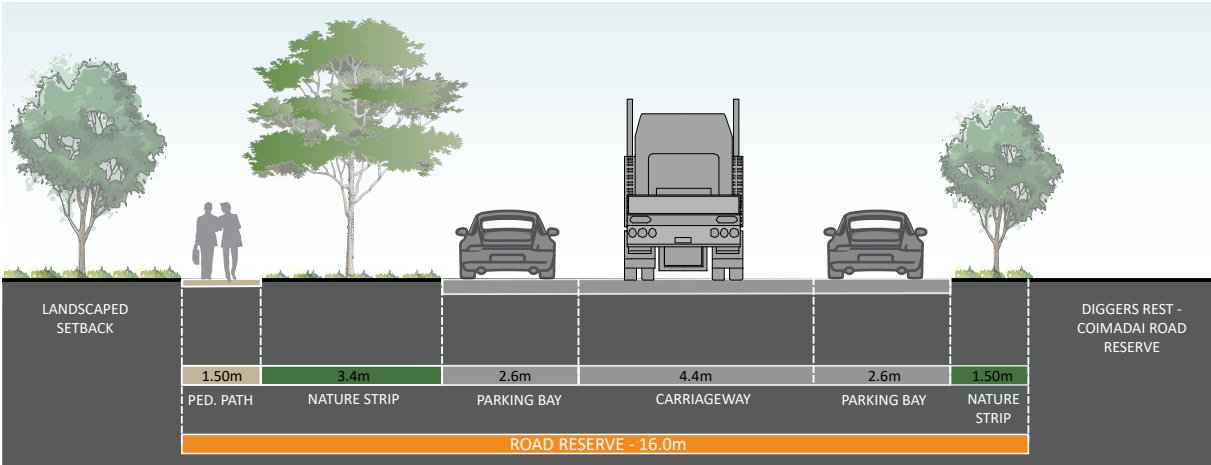
Figure 6 | Access and Movement Plan



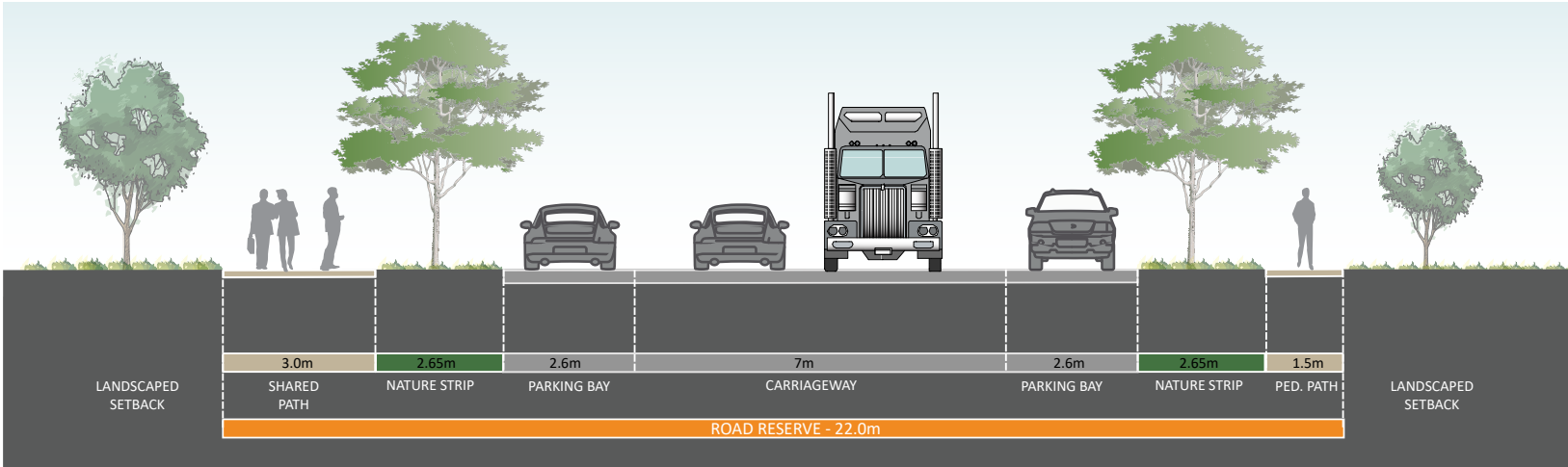
Road Cross Sections



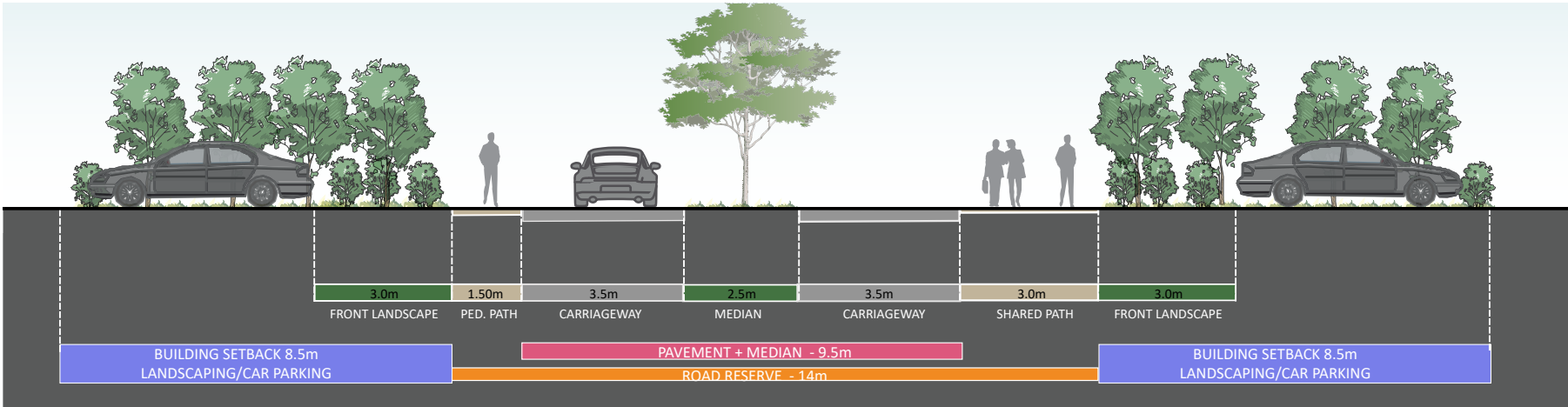
Cross Section 1 | Road A | 22m wide



Cross Section 4 | Service Road | 16m wide

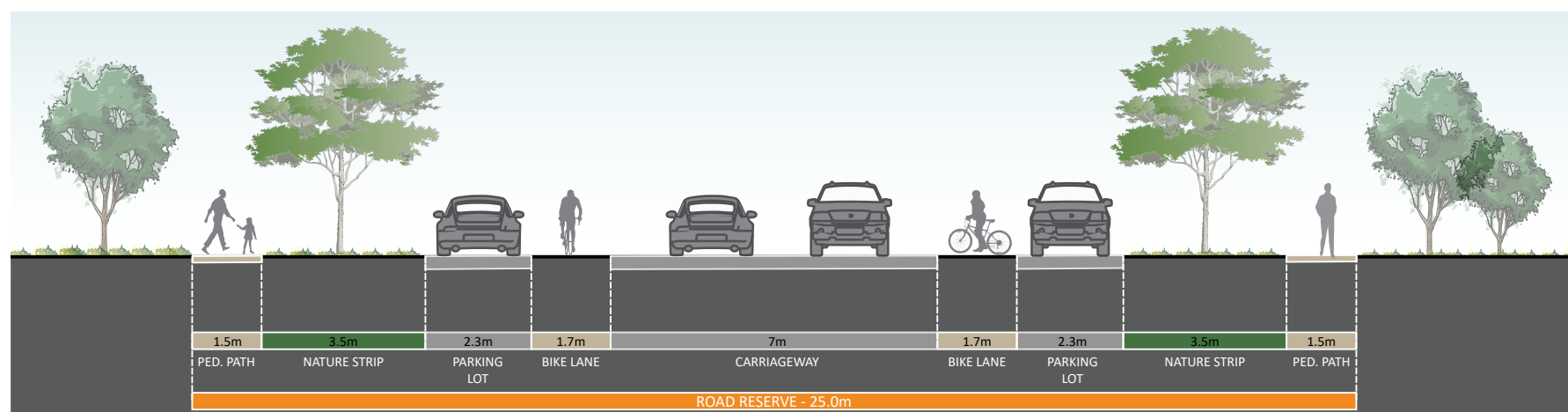


Cross Section 2 | Road B | 22m wide

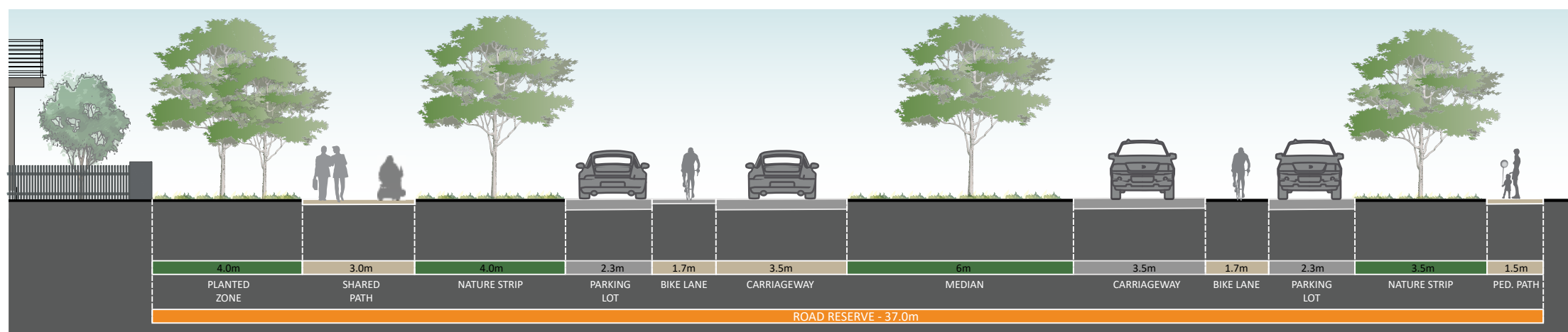


Cross Section 3 | Vehicle Access C | 31m wide reserve

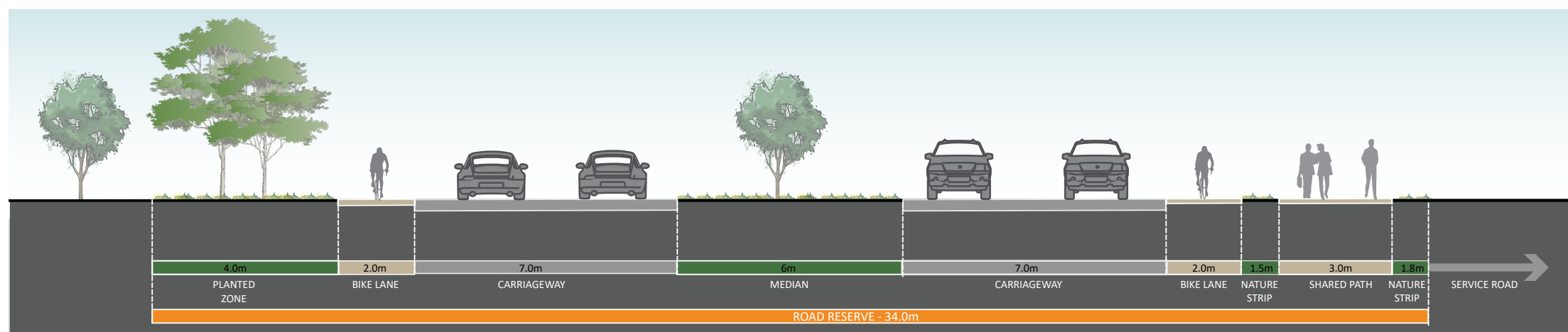
Road Cross Sections



Cross Section 5 | North South Connector Road to be delivered by others | 25m wide



Cross Section 6 | Residential Employment Divider with Shared Landscape Trail (License Road) to be delivered by others | 37m wide



Cross Section 7 | Arterial Road (Diggers Rest - Coimadai Road) to be delivered by others | 34m wide

08 Urban Grain and Employment

The employment precinct will provide a diversity of lot sizes and enable flexibility within allotments to cater for a range of industrial and business uses within a high amenity, visually attractive landscaped environment including uses such as office, manufacturing and service industry employment.

Principle

To ensure proposed land use mix fosters employment opportunities and protects the amenity of surrounding residential areas, at the License Road interface and the Green Wedge Zone to the south.

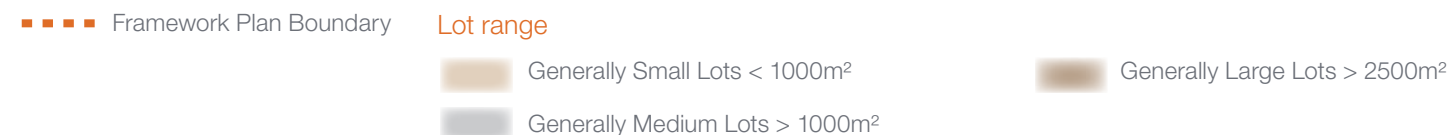
Guidelines

- 8.1 Provide a range of lot sizes to ensure a variety of uses within the employment precinct.
- 8.2 Provide lots that are regular in shape for efficient use of land.
- 8.3 Ensure container storage is not permitted on lots along License Road and north south Connector at the western boundary.
- 8.4 Locate small and medium sized lots along License Road to accommodate uses that will have a minimal impact on the amenity of the surrounding residential area.
- 8.5 Avoid uses that generate high traffic volumes and utilize large service vehicles along License Road.
- 8.6 Locate larger lots at Melbourne Water drainage reserve interface for industrial uses with active frontages.
- 8.7 Locate larger lots at Diggers Rest -Coimadai Road interface for manufacturing, industrial and or storage uses.
- 8.8 Ensure future use operating hours of proposed activities along License Road meet the required EPA noise requirements SN-1 to limit impact on neighbouring residential properties.
- 8.9 Ensure that the proposed uses along License road do not encroach on the minimum Threshold distance as prescribed in Clause 52.10 of the Melton Planning Scheme. The following uses are not permitted:

- Batching Plant
- Recycling Depot
- Transfer Depot
- Brothel
- Wrecking Yard
- Panel Beater



Figure 7 | Urban Grain and Employment Plan



Small Lot Warehouse uses



Small Lot Factoryette use



Medium Lot single use



Large Lot Office/Industrial uses



Large Lot Manufacturing uses

09 Development and Siting

Bloomdale estate residential subdivision is currently under development to the north of the employment precinct, it is imperative to ensure adequate visual and sound buffers are provided to this interface.

Principle

To ensure that the siting of the built form contributes to creating a consistent streetscape that is visually attractive and responds to surrounding context appropriately and positively contributes to the overall character of the Diggers Rest Township.

Guidelines - Building Orientation

- 9.1 Ensure lots and buildings address primary frontage.
- 9.2 Ensure customer service and or office component front primary street frontages to provide activation and clear access.
- 9.3 Ensure buildings are designed so that visitor entry points and office areas are clearly identifiable and face the primary street frontage.
- 9.4 Orientate corner lots to higher order street, including License Road, north south connector at western interface and Diggers Rest – Coimadai Road to promote activation.
- 9.5 Provide active building edges including glazing to secondary frontages on corner lots for passive surveillance.
- 9.6 Incorporate passive solar orientation where possible.

Guidelines - Building Setbacks

- 9.7 Ensure all buildings are setback a minimum of 9.0m from the primary street frontage.
- 9.8 Ensure the front building setback transitions from 12m to 9m on License Road across first four lots abutting Alexander Park Development to the east such that each subsequent lot from the UDF boundary loses one metre setback depth.
- 9.9 Ensure setback along secondary street frontage is minimum 3.0m. This setback must be landscaped. See Figure 8 for further detail.
- 9.10 Incorporate architectural features such as entry porticoes that protrude onto the front setback.
- 9.11 Ensure side setback provides for equitable development opportunity on both sides.
- 9.12 The maximum site coverage of all buildings should not exceed 70% of the site area.

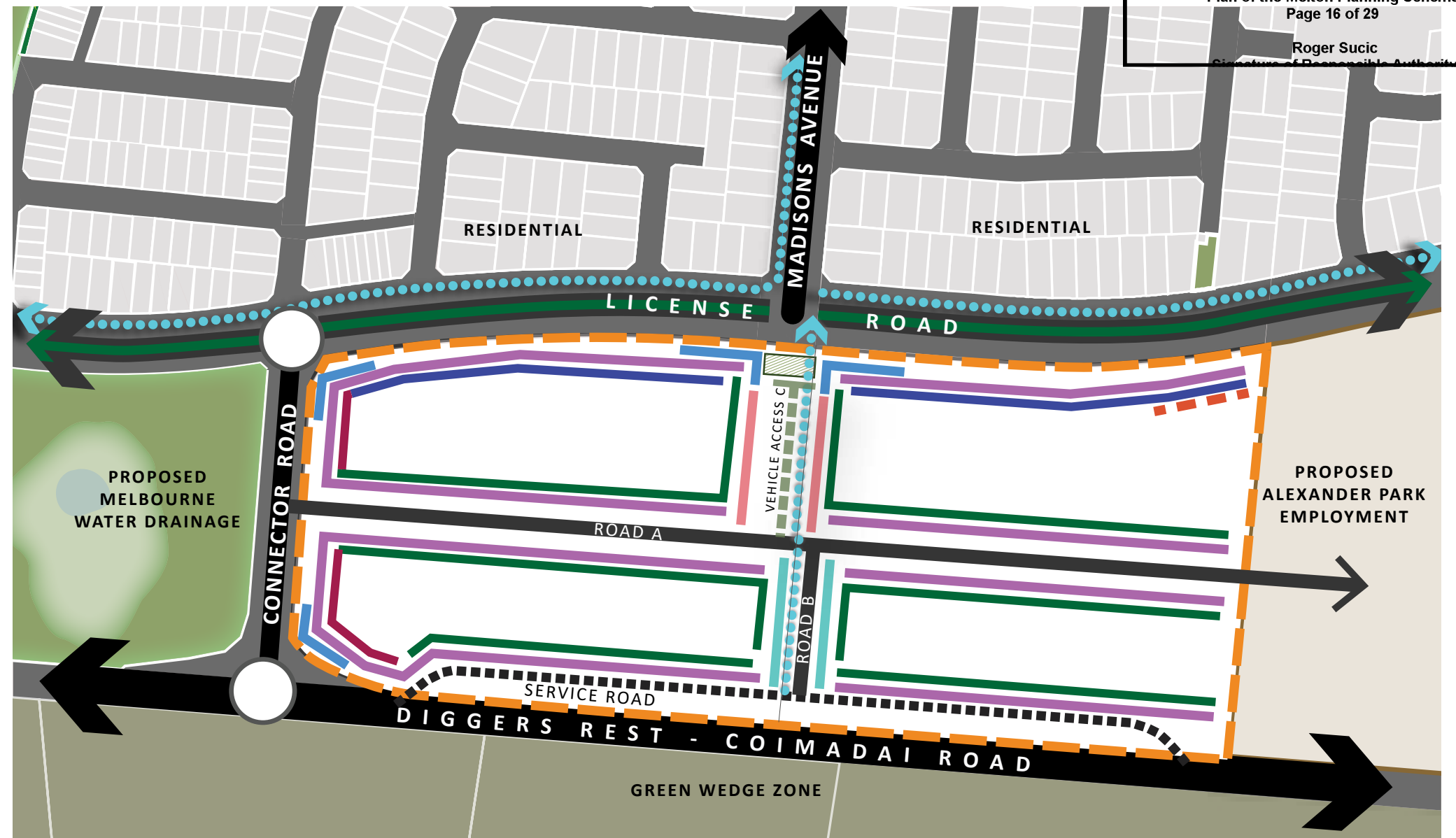


Figure 8 | Development Siting Plan

- | | | | |
|-----------------------------|--|---------------------------------|---------------------------|
| --- Framework Plan Boundary | --- Gateway corner treatment | --- Primary frontage 9m setback | --- 3m Landscape frontage |
| --- Landscaped Reserve | --- Secondary frontage 3m setback | --- 5m Landscape frontage | --- 6m Landscape frontage |
| --- Setback Transition | --- Refer Vehicle Access C (Section 3) for setback details | | |



Lots orientated to address the road



Office component at road frontage



Visitor and employee car parking



Secondary Frontage address *Image courtesy Nichols Crowder*

0 30 60 90 120 150m
1:3000 @A3

Guidelines - Landscape Setbacks

9.13 Ensure minimum 5.0m landscape setback for all lots along License Road.

9.14 Ensure the front landscape setback transitions from 9m to 5m on License Road across first four lots abutting Alexander Park Development to the east such that each subsequent lot loses one metre landscaping depth.

9.15 Ensure minimum 3.0m landscape setback for all lots along Roads A, Road B and Service Road.

9.16 Ensure minimum 6.0m landscape setback for all lots facing the Melbourne Water Drainage Reserve.

Guidelines - Car parking

9.17 Car parking, loading and service access should occur out of sight where possible.

9.18 On lots lesser than 1000m², car parking should be provided to the side and rear of the built form where practicable. Where this is not practicable, car parking may be provided in the front setback as per the standards of B10, of Councils Industrial Design Guidelines, with appropriate landscaping planting to provide screening to the front setback.

9.19 On lots greater than 1000m², car parking should be provided to the side and rear of the built form where practicable. Small areas of visitor car / disabled parking at front is considered appropriate provided that the front landscape setback is suitably planted and screened to the street.

9.20 Provide vehicle access from a single cross over for lots under 1500m². Refer to Melton City Council's industrial guidelines for further details.

9.21 Provide adequate on-site car parking for employees and visitors in accordance with Clause 52.06. Refer to Melton City Council off street car parking guidelines for requirements of landscaping within car parking areas.

9.22 Provide car parking to reflect the scale of activities undertaken on each site and in accordance with Clause 52.06 of the Melton Planning Scheme.

9.23 Include landscaping as a buffer/screen to car parking where it is located at the front of the site.

9.24 Provide appropriately detailed hard landscaping in the form of pedestrian walkways and footpaths from car parking areas to buildings.

9.25 Ensure sightliness from moving cars and between parked cars are not compromised for pedestrian safety.

9.26 Ensure adequate lighting within the car parking area for reasonable visibility of the surrounding area.

9.27 Ensure offices are designed to provide passive surveillance onto car parking areas with windows and entrances.

9.28 Encourage water sensitive urban design to be incorporated in car parking areas.

Guidelines - Loading

9.29 Integrate loading areas into the design of the building so that loading occurs internally.

9.30 Ensure loading areas are clearly separated from pedestrian and cyclist routes.

9.31 Ensure adequate and safe allocation of waste collection area.



Factoryette type development with shared front setback



Entry portico



WSUD incorporated in car parking



Office overlooking car parking



Landscaped front setback



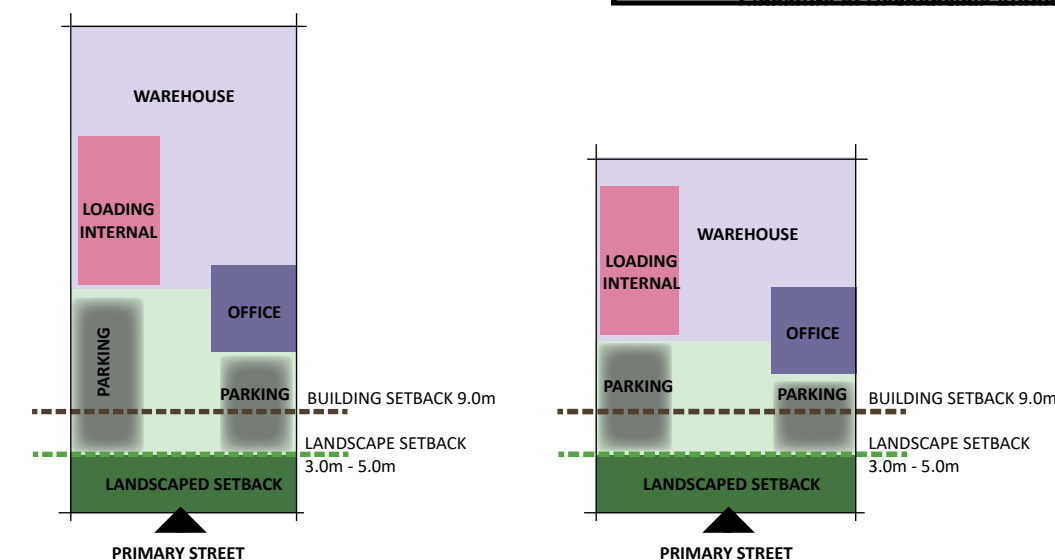
Landscape buffer to car parking



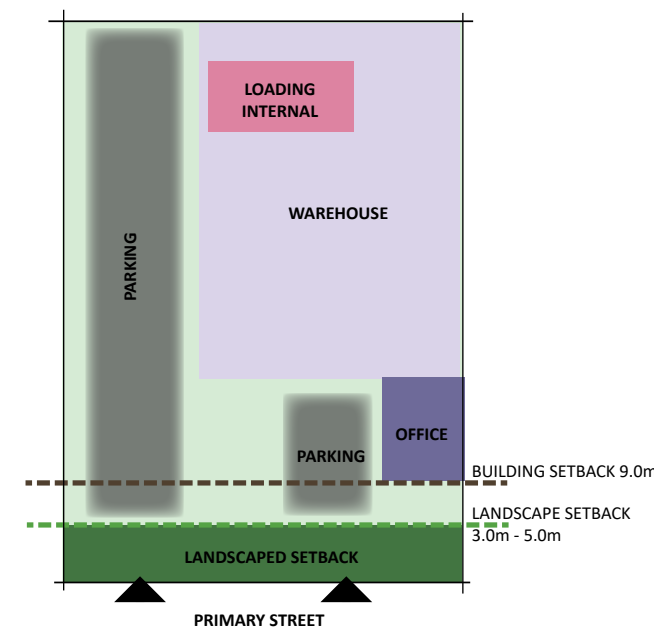
Pedestrian walkway separated



Loading integrated into the building



Typical Small Lot Siting < 1000m²



Typical Large Lot Siting > 1000m²

10 Built Form

The employment precinct is adjacent to residential area and hence the built form must be contextually responsive in scale and articulation. Gateway buildings on corner lots framing key entries into the site will provide visual interest and a sense of arrival.

Principle

To ensure that the development within the Employment Precinct is contextually responsive in terms of its form, scale, massing, articulation, roof profile and materials, and that these design elements collectively forge a sense of place and identity for the Employment Precinct.

Guidelines - Building Design

- 10.1 Energy efficient design and construction methods should be used for all buildings.
- 10.2 Buildings must be naturally ventilated to reduce reliance on artificial cooling and heating, where possible.
- 10.3 North and South facing large buildings should be designed to allow natural light to penetrate where possible.

Guidelines - Building Heights

- 10.4 Permit maximum building height 7.0m to the front of the lot at the end of the landscaped setback, this can increase to a maximum of 9.0m as per Melton Industrial Guidelines, at a further setback of 5.0m

Guidelines - Articulation

- 10.5 Orientate building entries to the street frontage for clear visibility and to activate the street frontage.
- 10.6 Locate pedestrian generating uses including customer service, retail and office components at the front of the street at minimum 40% height of the main building to reduce visual bulk.
- 10.7 Incorporate substantial window glazing facing the street frontage.
- 10.8 Avoid blank walls, feature treatments to include setbacks, varying heights and materials for articulation and to reduce visual bulkiness. Ensure that a blank wall does not exceed 10m in length.
- 10.9 Minimise visual bulk of Industrial Buildings through the co-location of offices, showrooms and/or customer service areas along street frontages.

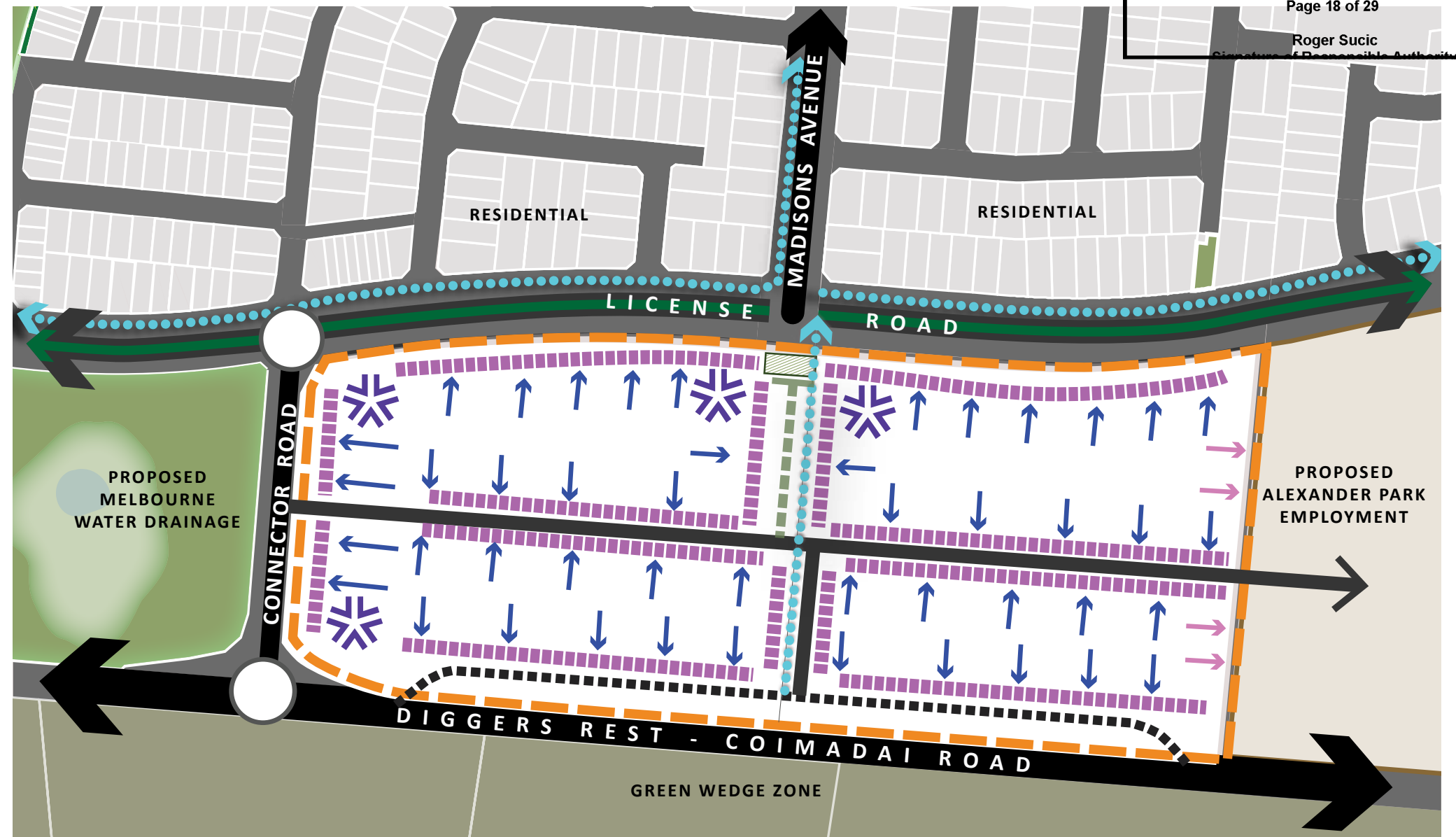


Figure 9 | Built Form Plan

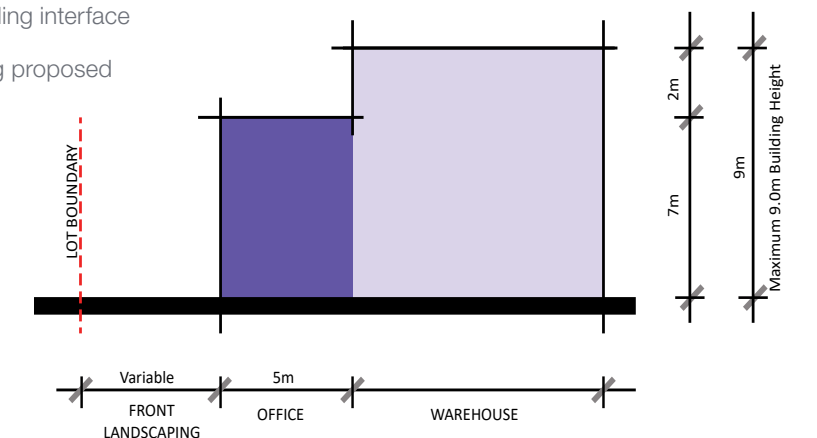
- Framework Plan Boundary
- ✱ Gateway sites
- ← Built form orientation to the surrounding interface
- Articulated frontage
- ← Built form responsive to the adjoining proposed employment estate



Building entry orientated to road frontage



Customer service to road frontage



Building height

- 10.10 Incorporate feature cladding, fenestration and glazing to avoid blank wall facades on the public realm.
- 10.11 Provide feature treatment to the corner addressing both the street frontages on corner lots.

Guidelines - Roof Form

- 10.12 Integrate the roof form into the overall design of the building.
- 10.13 Ensure the roof is low pitch and not bulky.
- 10.14 Utilise the roof form to differentiate and transition between the various elements of the building.
- 10.15 Locate mechanical building infrastructure behind screens utilizing roof forms or parapets that integrate with the overall design of the building.

Guidelines - Materials and Colours

- 10.16 Ensure all buildings address street frontages and where the side or rear building façades are visible from the public realm they should be design to provide variation in colour / materials and replicate primary façade treatments.
- 10.17 Ensure buildings are constructed from durable materials that maximise longevity.
- 10.18 Ensure external finishes are of low reflectivity.
- 10.19 Provide a palette of colours and finishes that are complementary and contribute to enhancing the overall presentation to the street. Avoid using primary colour palette along License Road interface and Melbourne Water Drainage interface, muted earthy tones which complement adjoining residential in Bloomdale are encouraged.
- 10.20 Consider the overall presentation of the materials, the extent, scale and proportion in context of the building and its application.
- 10.21 Ensure Colorbond is not used in excess of 30% of the building mass (excluding the roof), when visible from the street.
- 10.22 Consider utilising colours and materials:
 - Pre-cast concrete finished with coloured pigments, relief details, exposed aggregate, sand blasted finish, broom finish, artists moulds or attached artwork.
 - Textured paint and/or render.
 - Framed panel cladding, (Alucobond, or similar)
 - Corrugated metal sheeting with appropriate finishing (Colorbond, or similar)

- Insulated panels or composite panels with metal outer facing
- Perforated metal screens

Guidelines - Gateway Sites

- 10.23 Provide gateway treatments on lots identified in Figure 9. Gateway sites are located at street corners.
- 10.24 Incorporate feature cladding, façade articulation and colour variation to key gateway sites.
- 10.25 Ensure key gateway site is developed to have a ground floor active street frontage, incorporating substantial window glazing at street interfaces.
- 10.26 Ensure the building facades on side street and adjoining car parking areas do not exceed 10m without articulation, fenestration or visual interest.

Guidelines - Service, Storage and Display Areas

- 10.27 Ensure all outdoor storage is not visible from the street and should be located at the rear of property.
- 10.28 Locate screens strategically to conceal unsightly items and areas that do not contribute to the streetscape.
- 10.29 Ensure any facility requiring container storage is not located along License Road.
- 10.30 Storage containers should be screened or located on site such that it is not visible directly from a street.



Gateway Treatment



Stone cladding



Pre-cast concrete



Fenestration screen



Panel cladding



Metal awning



Screening around services



Articulated material use



Gateway Treatment



Building Edge Treatment

11 Public Realm

To complement the existing residential character the landscaping within the front setback must be high quality, requiring low maintenance and enhancing the overall presentation of the public realm.

Principle

To create a sense of place and identity by appropriate landscaping and streetscapes.

Guidelines - Landscaping

11.1 Utilise planting design with a restrained palette of complementary species within selected planting areas to provide a strong and cohesive identity for the development.

11.2 Ensure landscape design complements and enhances the architectural language of each building.

11.3 Consider the use of accent planting adjacent to Building Entrances to bring focus to that area and create a visual feature.

11.4 Utilise hedges to screen undesirable views and to control pedestrian access.

11.5 Ensure that the selected plant species are drought tolerant, either indigenous or exotic incorporating ground covers, low shrubs and trees.

11.6 Ensure plants are the dominant element within garden beds with species and densities that ensure full coverage of the garden bed area and minimise visibility of mulch areas.

11.7 Provide a high quality landscaped reserve at Vehicle Access C where it adjoins License Road.

11.8 Ensure design and construction of public realm area is in accordance with Council Policy, Guidelines and Standards.

11.9 Ensure passive irrigation of all trees and landscape elements is included unless otherwise agreed with the Responsible Authority.

11.10 Ensure streetscapes and pathways incorporate lighting that illuminates footpaths and pathways to ensure amenity and safety for users at night.

11.11 Ensure CPTED principles such as natural surveillance, controlled access and good maintenance are implemented in the design and construction of all public areas



Figure 10 | Public Realm Plan



Guidelines - Fencing

11.12 Incorporate fencing behind landscape setback area to lessen the visual impact of front fencing.

11.13 Proposed front fencing must not exceed 1.5m in height, all front fencing and side fencing on secondary interfaces must be constructed in a black steel post style allowing clear views. Pool style fencing is not permitted.

11.14 Barbed wire, razor wire or cyclone fencing must not be used.

11.15 Preferably, use landscaping rather than fencing to delineate the front boundary to provide a more open and attractive streetscape appearance.

11.16 High security fencing visible from the street frontage should only be used where necessary to screen outdoor storage areas that cannot locate out of public view and it must:

- Avoid the use of razor or barbed wire fencing, and
- Locate fencing at or behind the front building line to create an open and attractive street presence.

Guidelines - Car parking

11.17 Ensure that planting selection and design does not infringe upon sight lines for pedestrians and vehicles.

11.18 Provide landscaping in car parking areas incorporating passive irrigation systems.

11.19 Ensure impervious surfaces contain storm-water inlets to ensure treatment prior to legal discharge.

11.20 Provide sufficient future shade and canopy coverage for car parking areas.

11.21 Avoid trees with broad habits in locations where they may conflict with vehicular movements.

11.22 Utilise a minimum of 500mm width of prostrate planting / gravel treatment to accommodate door swing from parked cars to avoid conflict where garden beds are located immediately adjacent to car parks.

11.23 Consider Permeable paving where possible.

11.24 Ensure landscaping complies with Melton City Council off Street Car parking Guidelines.

Guidelines - Signage

11.25 Ensure signage does not detract from the façade or the streetscape and is not excessive in scale.

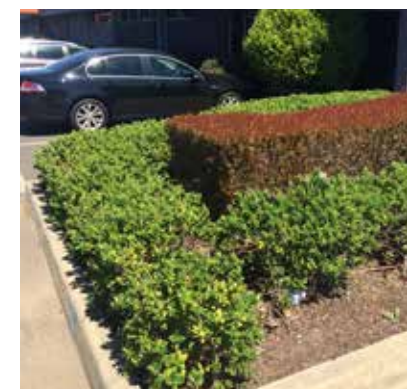
11.26 Ensure materials and lighting of the sign complements the building façade.

11.27 Ensure signage is not installed on the fencing or free standing on the pedestrian path.

11.28 Ensure all signage is incorporated within the building design, is in keeping with the architectural form of the building and is of a scale respectful of the surrounding context. Freestanding and roof top signage is not permitted

11.29 Signage must accord with Schedule 52.02 of the Melton Planning Scheme.

11.30 Ensure signage is provided in designated signage areas.



Planting Palette



Canopy tree in parking area



Landscaped front setback



Front fencing



Pedestrian path separated from parking



Signage



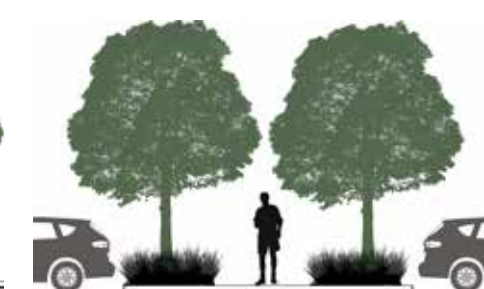
Front landscaping



Planting to accommodate door swing



Planting at property boundary below eye level



Separated pedestrian path

12 Interface Treatments

The employment precinct is bound by License Road to the north and future employment subdivision to the east. Madisons Avenue connects the precinct at the centre of its northern interface with the future Town Centre. Diggers Rest – Coimadai Road forms the southern interface and connects the precinct to Diggers Rest train station. The built form and streetscapes must respond to these interfaces.

Principle

To provide active and articulated frontages to appropriate interfaces to create a distinct identity to the Employment Precinct without compromising the overall identity of Diggers Rest Township.

Guidelines - License Road Interface

- 12.1 Ensure container storage is not permitted on lots along License Road.
- 12.2 Locate small and medium sized lots along License Road to accommodate uses that will have a minimal impact on the amenity of the surrounding area. Manufacturing and industrial uses are not permitted along License Road.
- 12.3 Orientate corner lots to License Road to promote activation.
- 12.4 Provide a 5m landscape setback to ensure that the industrial interface along License Road is screened from public view via more intense landscaping than typical for industrial sites.
- 12.5 Ensure the front landscape setback transitions from 9m to 5m on License Road across first four lots abutting Alexander Park Development to the east such that each subsequent lot loses one metre landscaping depth.
- 12.6 Permit maximum building height 7.0m to the front of the lot at the end of the 5.0m landscaped setback, this can increase to maximum of 9.0m at a further setback of 5.0m along License Road.
- 12.7 Ensure future use operating hours meet the required EPA noise requirements SN-1 to limit impacts on neighbouring residential properties.
- 12.8 Avoid using primary colour palette along License Road interface and Melbourne Water Drainage interface, muted earthy tones which complement adjoining residential in Bloomdale are encouraged.
- 12.9 Ensure illuminated signage is not visible from License Road and also ensure that no illuminated signs are provided adjacent to residential or community uses.



Figure 11 | Interface Treatments Plan

- Framework Plan Boundary
- License Road Interface
- Melbourne Water Drainage Reserve Interface
- Connect to the shared path network
- Diggers Rest - Coimadai Road Interface
- Proposed Alexander Park Employment Interface

- 12.10 Landscaping within the front setbacks for lots along License Road must achieve the following:
- ensure a minimum of 2 to 3 large canopy trees are planted, aiming to reduce the head island effect;
 - ensure the planting, in appropriate densities, of lower height vegetation transitioning to higher height screen planting closer to the built form.

Guidelines - Diggers Rest - Coimadai Road Interface

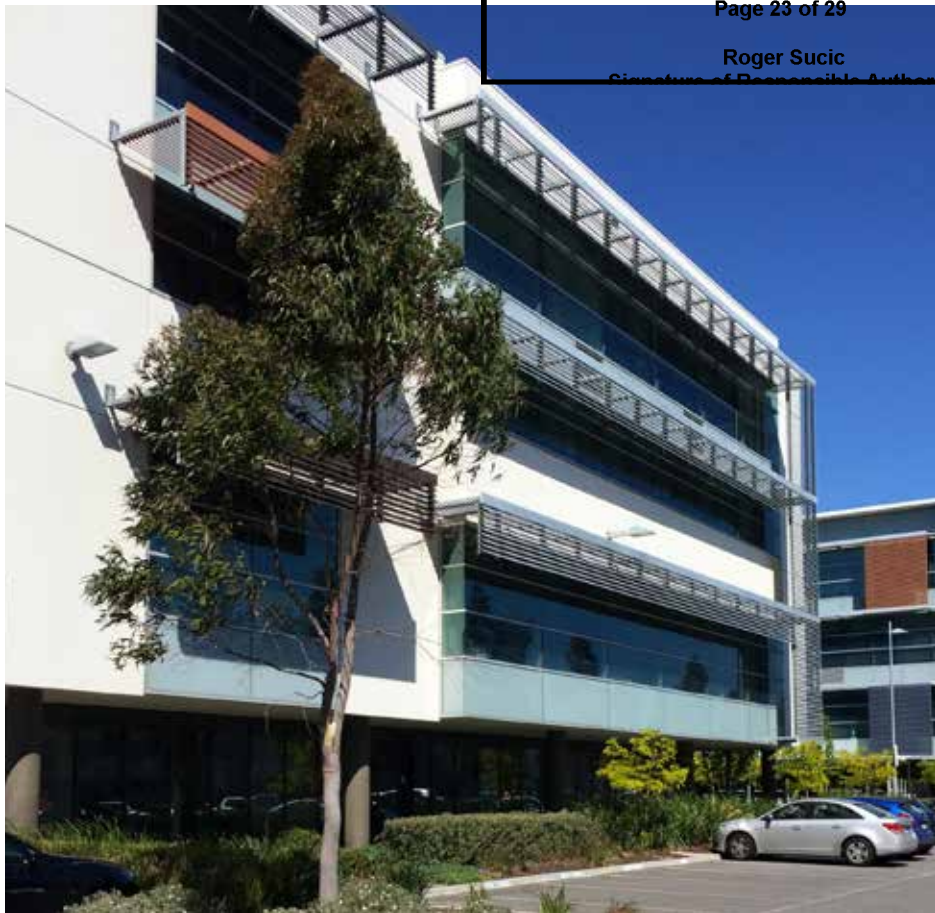
- 12.11 Locate large lots at the Diggers Rest -Coimadai Road interface for manufacturing, industrial and or storage uses.
- 12.12 Orientate corner lots to the higher order Diggers Rest – Coimadai Road to promote activation.
- 12.13 Provide car parking and loading areas to the side or rear of the site where possible.
- 12.14 Provide a landscaped setback, which includes large canopy tree planting along the southern boundary with existing rural properties.

Guidelines - Melbourne Water Drainage Reserve Interface

- 12.15 Ensure container storage is not permitted on lots along north south connector road unless screened.
- 12.16 Locate large lots at Melbourne Water drainage reserve interface for industrial uses with active frontages.
- 12.17 Orientate corner lots to north south connector at Melbourne Water drainage reserve interface to promote activation.
- 12.18 Provide opportunities for passive surveillance of open space through placement of windows, doors and/or active uses along frontages.
- 12.19 Crossovers should be located to maximise on-street car parking opportunities opposite the drainage reserve.
- 12.20 Permit maximum building height 7.0m to the front of the lot at 6m setback, this can increase to maximum of 9.0m at a further setback of 5m along north south connector at the Melbourne Water drainage reserve interface.
- 12.21 Provide a landscaped setback, which includes large canopy trees at the Melbourne Water drainage reserve interface.
- 12.22 Provide car parking and loading areas to the side or rear of the site where possible.



Potential presentation along License Road Interface



Potential presentation along Melbourne Water Drainage Interface



Potential presentation along Melbourne Water Drainage Interface



Potential presentation along Diggers Rest - Coimadai Road Interface

13 Maintenance

Principle

To ensure that the Employment Precinct is reliant on good practice operational maintenance derived from robust sustainability principles.

Guidelines - Site Maintenance

13.1 Ensure all buildings, signage, landscaping, car parking, paths and paved areas are appropriately maintained to a high standard.

13.2 Ensure physical services are available and connected prior to occupation of any buildings.

13.3 Ensure any chemicals or waste products are appropriately contained within lot boundaries and must not discharge onto adjoining land.

13.4 Stormwater drainage design should incorporate Environmentally Sustainable Design principles and provide rainwater runoff reuse for landscaping irrigation where appropriate.

Guidelines - Waste Storage and Disposal

13.5 Site management strategies should incorporate waste management considerations.


13.6 Ensure waste storage and disposal is integrated within the site and/or building design.

13.7 Ensure recycling storage facilities and waste awaiting collection is stored in a purpose- built facility, screened from street frontages and staff amenity areas.

13.8 Ensure that waste recycling and reuse is maximised.

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CITY OF



MELTON

MELTON PLANNING SCHEME

Urban Design Framework approval in accordance with
Clause 37.07 - Schedule 5 - Dggers Rest Precinct Structure
Plan of the Melton Planning Scheme.
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Roger Sucic
Signature of Responsible Authority



Appendix - Diggers Rest Precinct Structure Plan Assessment

#	PSP Requirement	Response
R37	An Urban Design Framework Plan must be prepared and approved by the Responsible Authority prior to the use, development and subdivision of the employment land	The Bloomdale Employment Precinct – Urban Design Framework (20.05.2020) meets the requirement of
R38	<p>The Urban Design Framework Plan must:</p> <ol style="list-style-type: none"> 1. Address key view lines and sight lines into and out of the area and incorporate within the overall design. 2. Locate manufacturing and industrial uses with adverse amenity potential at suitable distances from residential interfaces and incorporate management measures where required. 3. The interface with road network will: <ul style="list-style-type: none"> • create of a high amenity, visually attractive environment conducive to the development of built form with active and articulated frontages, including uses such as office and manufacturing and service industry employment • encourage the location of gateway buildings and landscape features at the Vineyard Road intersections and key vistas 	<ol style="list-style-type: none"> 1. The UDF has identified site lines and connections to the surrounding land uses. Specifically, Figure 4 of the UDF identifies the framework for the subject site provides for opportunities for sight lines from Madison Ave to be retained and continue through the development to the Green Wedge Zone area located to the south of the subject site. <p>Furthermore, Figure 5 within the UDF illustrates the key visual connections into and out of the subject site. These include from the Melbourne Water Drainage reserve, from and into the Green Wedge Zone and from and into the site near the Madison Ave and Licence Road intersections.</p> <ol style="list-style-type: none"> 2. The proposed layout and proposed lot sizes, identified within Figure 7 of the UDF have been designed in such a way so as to discourage industrial uses with adverse amenity potential from locating adjacent to the residential land uses to the north. This has been achieved by



MELTON PLANNING SCHEME

Urban Design Framework approval in accordance with Clause 37.07 - Schedule 5 - Diggers Rest Precinct Structure Plan of the Melton Planning Scheme.

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#	PSP Requirement	Response
	<ul style="list-style-type: none"> provide a visually attractive landscaped environment along roads leading to and within the employment land. <p>4. Show how the employment land relates to and responds positively to the adjacent activity centre and residential land through high quality urban design treatments.</p> <p>5. Set out measures to avoid long blank walls with minimal visual interest along road frontages</p>	<p>providing smaller lots, resulting in niche development potential which will likely result in more sensitive land uses being located within proximity to the residential land. Furthermore, the guidelines provided within Section 8 of the UDF provide guidance in relation to the types of uses that are and are not appropriate, acceptable EPA noise guidelines and operating hours, and minimum threshold distances as per the Melton Planning Scheme.</p> <p>3. Section 9, 10, 11 and 12 of the UDF provide guidelines to ensure a high-quality built form is achieved, with high amenity values for the future industrial estate. Specific guidelines are provided in relation to the following items, which is considered appropriate to ensure an attractive and high amenity employment environment:</p> <ul style="list-style-type: none"> Setbacks Landscaping setbacks Carparking Loading areas Built form and design Building heights Articulation, colours and materials Landscaping external to the site Fencing Signage Storage <p>4. Section 8, 9, 10 and 11 of the UDF have been devised in a holistic fashion to ensure that a combined approach is taken utilising a</p>

#	PSP Requirement	Response
		<p>number of the tools within the urban design tool kit to ensure an appropriate and sensitive interface is provided to the existing residential land uses to the north of the subject site.</p> <p>Specifically, the layout of the subject site has been developed with smaller lots fronting to License Road in order to discourage non-sensitive land uses from this location. Furthermore, significant guidelines have been included seeking to ensure that a combination of minimum setbacks, minimum landscaping requirements and maximum building heights, coupled with design guidelines requiring articulation, variations in colours and materials it is considered that the UDF adequately addresses this requirement.</p> <p>5. Section 10 of the UDF specifically relates to providing guidelines for the Built Form. Guideline 10.8 states:</p> <p><i>Avoid blank walls, feature treatments to include setbacks, varying heights and materials for articulation and to reduce visual bulkiness. Ensure that a blank wall does not exceed 10m in length</i></p> <p>Guide 10.10 states:</p> <p><i>Incorporate feature cladding, fenestration and glazing to avoid blank wall facades on the public realm</i></p> <p>In addition to the above specific guidelines the UDF provides direction on the use of different colours and materials to ensure that visual interest is created to street frontages.</p>